



THRUSH AIRCRAFT, INC.

P.O. Box 3149
300 Old Pretoria Rd.
Albany, GA 31706-3149
Product Support
229-883-1440 ext. 341
FAX 229-436-4856
jbays@thrushaircraft.com

SERVICE BULLETIN

No. SB-AG-50

April 15, 2009

PT6A-60AG P3 LINE CHAFING

THIS SERVICE BULLETIN AFFECTS THOSE AIRPLANES THAT
HAVE A PT6A-60AG ENGINE INSTALLED

AIRPLANES AFFECTED:

<u>MODEL</u>	<u>SERIAL NUMBERS*</u>
S2R-T660	T660-001 thru T660-008
S2RHG-T65	T65HG-011 thru T65HG-013
S2RHG-T65	T65HG-016
S2RHG-T65	T65HG-017DC
S2RHG-T65	T65HG-019DC
S2RHG-T65	T65HG-021DC
S2RHG-T65	T65HG-022DC

*Serial numbers listed above may be with or without the DC suffix.

REASON FOR PUBLICATION:

A case has been reported of the P3 line on a Pratt & Whitney PT6A-60AG engine chafing on the aft side of the lower right hand (as seen from cockpit) engine mount bracket. The chafing had nearly worn through the P3 line.

The P3 line goes from the low pressure compressor case to the fuel control, indicating to the fuel control how much air is flowing through the engine, and thus how much fuel is required. A leak in the P3 line will cause the fuel control to sense less airflow through the engine, causing it to reduce the fuel supply. Complete loss of P3 line pressure will cause the engine to go to minimum fuel flow and be unresponsive to throttle inputs.

COMPLIANCE:

The inspection portion of this Service Bulletin must be complied with immediately upon receipt or before further flight. If required, the repair portion of this Service Bulletin must be accomplished prior to further flight.

BY WHOM WORK WILL BE ACCOMPLISHED:

FAA licensed A&P mechanic or equivalent:

APPROVAL:

This Service Bulletin is FAA approved

MAN HOURS:

1 hour for access and inspection, 1 hour for modifying the engine mount bracket, 1 hour for part replacement, and 1 hour to return the airplane to flight status and make the appropriate log book entry and notify Thrush Aircraft, Inc. of Service Bulletin .compliance.

SPECIAL TOOLS:

None

INSPECTION:

Remove cowl panels and inlet plenum panels sufficient to gain access to the aft side of the lower right engine mount bracket.

Locate the P3 line attached to the engine case slightly aft of the lower right engine mount bracket (ref. Figure 50-1). The P3 line runs circumferentially counter-clockwise from the engine case pressure tap, turns aft through the fireseal and then goes to the fuel control.

Because of the location of the P3 line pressure tap on the engine case, the P3 line passes close to the aft side of the lower right engine mount bracket (specifically the stiffening flanges pointing aft).

Check to see that the P3 line is no closer than .20" to any part of the lower right engine mount bracket. If the P3 line meets this requirement, you need only log compliance with Thrush Aircraft, Inc. Service Bulletin SB-AG-50 in the airplane maintenance log and fill out and mail the compliance record back to Thrush Aircraft, Inc.

There is no repetitive inspection.

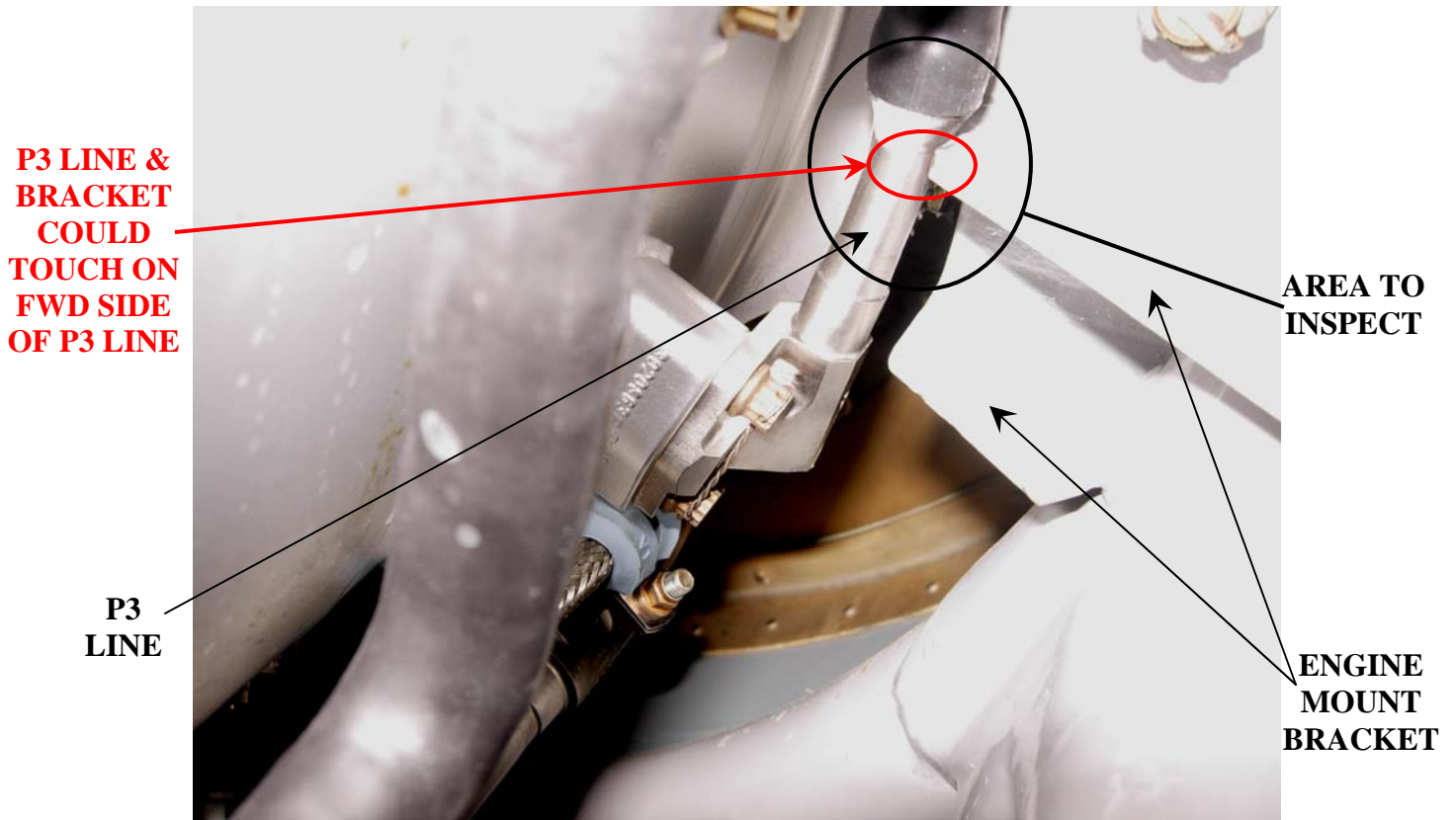


FIGURE 50-1: P3 Line and Lower Right Engine Mount Bracket
View looking up from behind mount ring

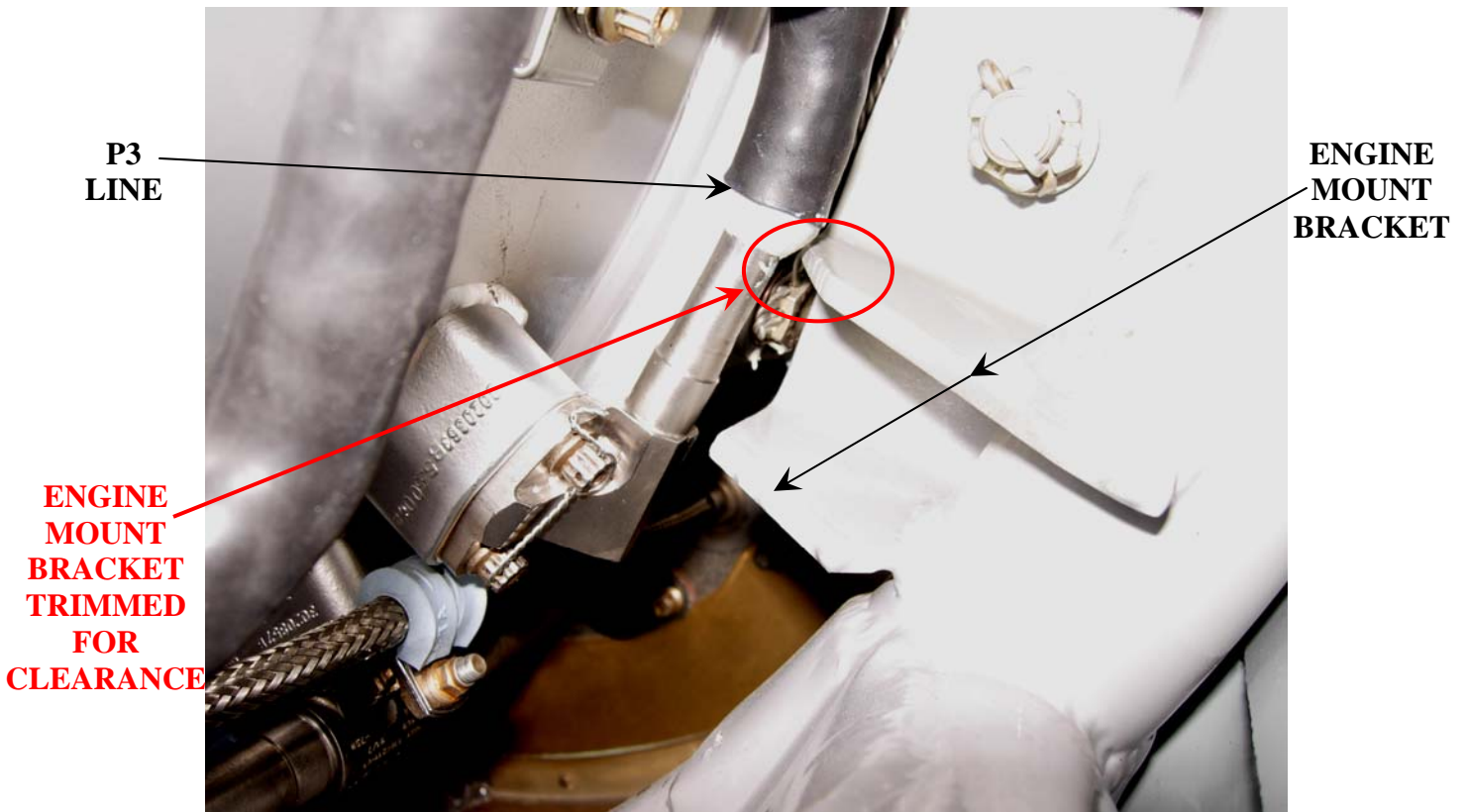


FIGURE 50-2: Engine Mount Bracket Trimmed to Clear P3 Line

REPAIR:

If the clearance between the P3 line and any part of the lower right engine mount bracket is not .20”, the bracket must be trimmed to gain this clearance (see Figure 50-2). Be careful not to damage the P3 line. It is recommended that the P3 line be removed before trimming the bracket. Check clearance after trimming, and when clearance is .20” or better prime and paint the exposed metal.

If the P3 line has been in contact with the engine mount bracket there will be evidence of that on the P3 line. If the damage is more than burnishing the line (ie. it is gouged or deformed), the line must be replaced before further flight.

PARTS:

The P/N for the PT6A-60AG P3 line is 3071785-01 or 3045748-01. They are interchangeable.

It can be purchased from

**Pratt & Whitney Component Solutions
24 / 7 Service**

Telephone: (800) 872-1792
Fax: (231) 799-8732

RECORD OF COMPLIANCE

Make appropriate entry in airplane maintenance records as follows: “Thrush Service Bulletin SB-AG-50 inspection requirement complied with at _____ total hours on aircraft.” and one of the following statements:

“There was already sufficient clearance between the lower right engine mount bracket and the P3 line.” or “The engine mount bracket was trimmed to provide .20” minimum clearance between the bracket and the P3 line.” or “The engine mount bracket was trimmed per SB-AG-50 and the P3 line was replaced.”

The mechanic must sign off this statement in the aircraft’s maintenance records (example below).

by: John A. Doe 1234567 June 31, 2009
name & certificate # date

RESPONSE CARD

The final step in compliance with this Service Bulletin is to complete and return the compliance card on the next page. If this SB is printed double sided, your personal information is hid when folded. The response card may be mailed, Faxed, or scanned and e-mailed.

FAX to: Greg Moreland 229-436-4856

E-mail to: Greg Moreland gmoreland@thrushaircraft.com

Service Bulletin SB-AG-50 Compliance Report

Aircraft S/N:	_____	Aircraft Owner:	_____
Aircraft Registration #	_____	Address of Owner:	_____
Airframe total time:	_____	City & State:	_____
Engine total time:	_____	Physical location:	_____
Date of Compliance	_____		_____
Complied with by:	_____	Certificate #:	_____
Signature:	_____		_____

PLEASE RETURN THIS REPORT ONLY AFTER THE SB IS COMPLETED

This response card may be mailed, Faxed to QC Manager at (229) 436-4856, or e-mailed to gmoreland@thrushaircraft.com.

Fold UP so address shows, tape & mail (Do Not Staple)

Service Bulletin No SB-AG-50

4/15/2009

THRUSH AIRCRAFT INC.
Attn: Greg Moreland
P.O. Box 3149
Albany, GA 31706-3149

Return Address
